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ARCADIS  
1400 PARKSIDE DR STE 410  
KNOXVILLE, TN 37934

RACHEL H. COLE, P.E. NO. 119667

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET .....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS .....	1A
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E
TABULATED QUANTITIES .....	2F
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1
TRAFFIC CONTROL PLANS .....	T2

YEAR	PROJECT NO.	SHEET NO.
2025	NH-I-75-1(159)	ROADWAY-SIGN 1
	541075-F8-005	

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SIGNATURE  
SHEET

Index Of Sheets  
SEE SHEET NO. 1A

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

MCMINN COUNTY

I-75:  
FROM SOUTH OF COUNTY ROAD 28 (L.M. 2.06)  
TO SOUTH OF COUNTY ROAD 50 (L.M. 4.32)

RESURFACE  
NIGHT WORK, COLD PLANING, GR CM MIX, OGFC,  
INTELLIGENT COMPACTION, AND PAVEMENT MARKINGS  
STATE HIGHWAY NO. I-75 F.A.H.S. NO. N/A

54I075-F8-005  
END PROJECT NO. NH-I-75-1(159) RESURFACE  
M.M. #39.17 L.M. 4.32

54I075-F8-005  
BEGIN PROJECT NO. NH-I-75-1(159) RESURFACE  
M.M. #36.94 L.M. 2.06

ACTIVE PROJECT CONTRACT NO. CNY261  
I-75: FROM NORTH OF HOOPER GAP ROAD NW TO SOUTH OF COUNTY ROAD 28

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER: ANDREW ZAZZARA, P.E.

DESIGNED BY : ARCADIS

DESIGNER : NATALIE KERNISANT CHECKED BY RACHEL H. COLE, P.E.

P.E. NO. 98023-4113-04 (DESIGN)

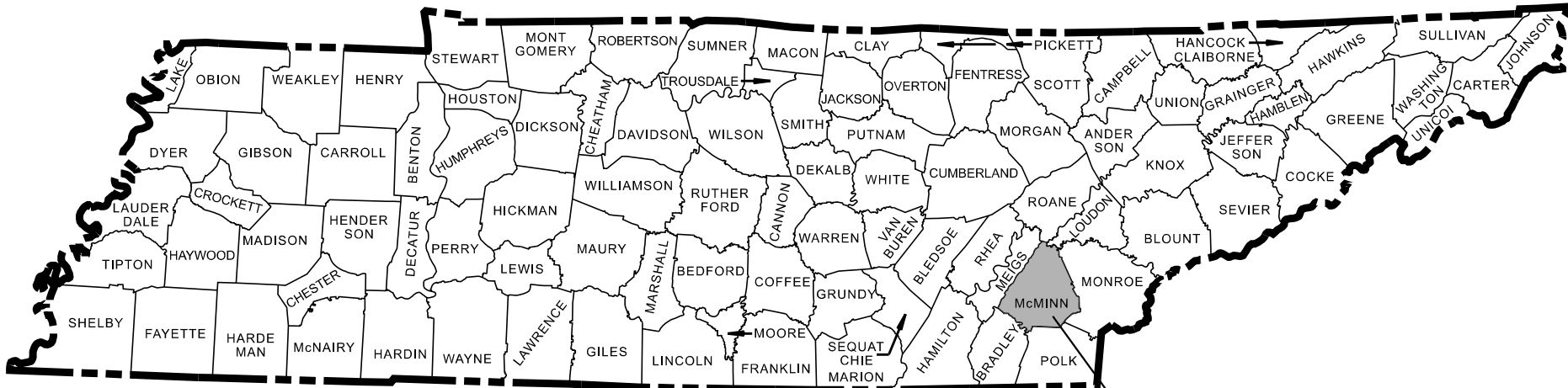
PIN NO. 135067.00

SCALE: 1"= 5280'

PROJECT LENGTH 2.26 MILES  
TOTAL LANE MILES RESURFACED 9.04 MILES

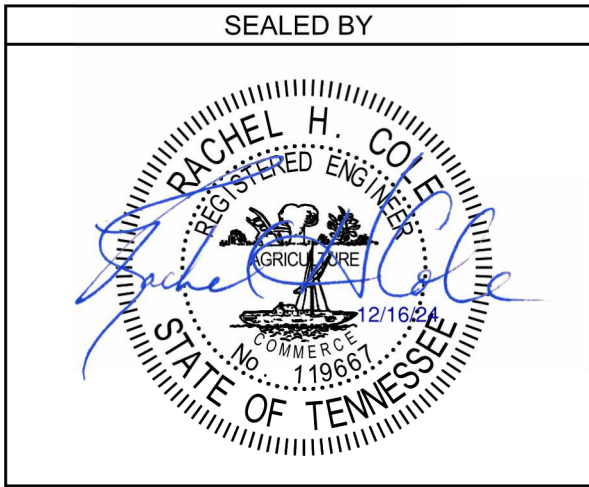
DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR 2025	SHEET NO. 1
FED. AID PROJ. NO.	NH-I-75-1(159)	
STATE PROJ. NO.	54I075-F8-005	



PROJECT LOCATION  
BRIDGE ID. # 54I00750035, 54I00750003, 54I00750004, 54I00750005, 54I00750006

NO EXCLUSIONS



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:   
DIVISION ADMINISTRATOR DATE

TRAFFIC DATA	
ADT (2025)	42973
POSTED SPEED	70 MPH



ROADWAY INDEX

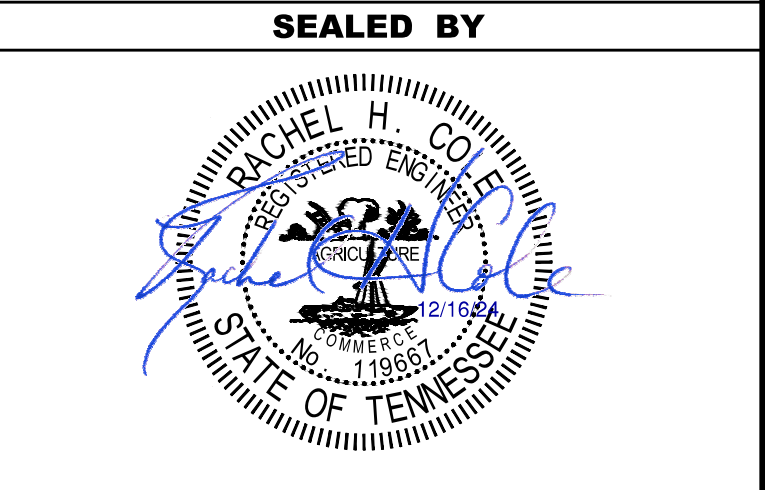
STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
SIGNATURE SHEETS.....	ROADWAY-SIGN1	<b>10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS</b>		
TITLE SHEET .....	1			
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A	RD-TP-1	10-01-24	STANDARD ROADWAY DRAWINGS TITLE SHEET
ESTIMATED ROADWAY QUANTITIES .....	2	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
GENERAL NOTES.....	2C	RD-L-1	02-20-20	STANDARD LEGEND
SPECIAL NOTES.....	2D	RD-L-1A		STANDARD LEGEND
ENVIRONMENTAL NOTES.....	2E			
TABULATED QUANTITIES .....	2F	<b>10-107.00 DESIGN - TRAFFIC CONTROL</b>		
UTILITY NOTES AND UTILITY OWNERS.....	3	T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1	T-M-5	03-01-23	MARKING DETAIL FOR FREEWAYS
TRAFFIC CONTROL PLANS .....	T2	T-M-8	06-28-19	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
		T-M-15	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
		T-M-18	10-29-21	FLEXIBLE DELINEATOR DETAILS
		T-M-18A		DELINEATOR MOUNTING DETAILS
		T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
		T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
		T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
		T-WZ-15	05-01-20	INTERIOR LANE CLOSURE FOR FREEWAYS
		T-WZ-21	05-01-20	LANE CLOSURE WITH LEFT HAND MERGE AND LANE SHIFT
		T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
		T-WZ-65		LANE CLOSURE WITH LATE MERGE
		T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.

NO PROJECT COMMITMENTS INCLUDED IN THIS PROJECT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-L-75-1(159)	1A
		541075-F8-005	



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

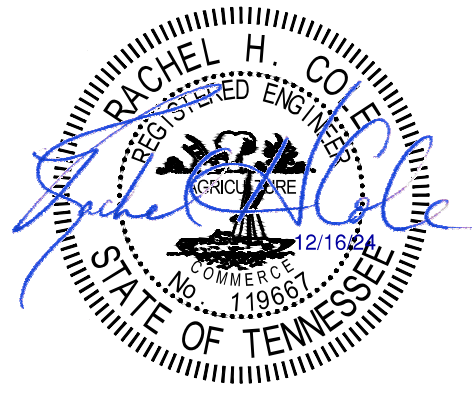
ROADWAY INDEX  
AND  
STANDARD  
ROADWAY  
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-L-75-1(159)	2
		541075-F8-005	

ESTIMATED ROADWAY QUANTITIES				
ITEM NO.		DESCRIPTION	UNIT	QUANTITY 541075-F8-005
(1)(2)	208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	9.1
(3)	303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	597
(4)	307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	120
	307-01.18	ASPHALT CONCRETE MIX (PG64-22) GRADING CM	TON	2407
(5)	307-03.12	ASPHALT CONCRETE MIX (PG76-22) GRADING CM	TON	6246
	403-01.10	HIGH PERFORMANCE FOG SEALS	S.Y.	10607
	411-03.10	ACS MIX (PG76-22) GRADING D	TON	224
(6)	411-03.23	ACS MIX (PG76-22) OGFC	TON	5740
(2)	411-12.01	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	9.1
	415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	8604
(7)	712-01	TRAFFIC CONTROL	LS	1
	712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	300
	712-05.01	WARNING LIGHTS (TYPE A)	EACH	150
(8)	712-06	SIGNS (CONSTRUCTION)	S.F.	1771
	712-08.03	ARROW BOARD (TYPE C)	EACH	2
	712-08.08	SPEED FEEDBACK SIGN ASSEMBLY	EACH	2
(9)	712-08.09	DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	4
	712-08.10	MOBILE MESSAGE SIGN UNIT W/ATTENUATOR	HOURL	500
(10)	712-08.12	QUEUE PROTECTION TRUCK	DAY	42
	713-02.11	REMOVAL OF EXISTING DELINEATORS	EACH	596
(11)	713-02.14	FLEXIBLE DELINEATOR (WHITE)	EACH	596
	713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	2
	716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	5364
(12)(13)	716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	5364
(14)	716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	11.3
(15)	716-12.02	ENHANCED FLATLINE THERMO PVM TMRKNG (6IN LINE)	L.M.	11.3
(15)	716-12.09	ENHANCED FLAT LINE THERMO (12IN LINE)	L.F.	8040
	717-01	MOBILIZATION	LS	1
		ALTERNATE AA-1		
(16)(17)(18)	403-02.01	TRACKLESS TACK COAT	TON	135
		ALTERNATE AA-2		
(18)	403-02.01	TRACKLESS TACK COAT	TON	45
(19)	403-02.02	HOT APPLIED TACK COAT	TON	67

FOOTNOTES	
(1)	INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK.
(2)	INCLUDES 4.55 L.M. INSIDE SHOULDERS AND 4.55 L.M. OUTSIDE SHOULDERS.
(3)	ITEM FOR LOW SHOULDERS AS DIRECTED BY TDOT PROJECT ENGINEER. (QUANTITY BASED ON 23,824' LENGTH, 2' WIDTH, AND 2" DEPTH.).
(4)	TO BE USED AT BRIDGE ENDS. TO BE USED AS DIRECTED BY THE TDOT PROJECT ENGINEER.
(5)	INCLUDES 120 TONS FOR THE CENTER CROSSOVER AT LM 4.040.
(6)	92 TONS TO BE USED FOR PLANT STARTUP AND HEATING UP EQUIPMENT AT BEGINNING OF EACH SHIFT, 15 TONS WASTE MATERIAL PER DAY.
(7)	INCLUDES ALL COSTS ASSOCIATED WITH NIGHTTIME WORK ZONE LIGHTING.
(8)	SEE SHEET 2F FOR SIGN TABULATED QUANTITIES. THE CONSTRUCTION SIGNING IS TO A MINIMUM. OTHER SIGNS MAY BE REQUIRED AS DIRECTED BY THE TDOT MANAGER.
(9)	SEE SHEET T2 FOR SUGGESTED PLACEMENT DETAILS.
(10)	QUANTITY WILL PROVIDE QUEUE PROTECTION FOR BOTH NIGHTLY PAVING OPERATIONS AND WEEKEND CLOSURES FOR THE BRIDGE WORK AS DIRECTED BY THE TDOT PROJECT ENGINEER.
(11)	QUANTITY BASED ON 50% OF TOTAL QUANTITY TO REPLACE MISSING OR DAMAGED WHITE FLEXIBLE DELINEATORS. SEE SPECIAL NOTES - FOG ZONE ON SHEET 2D FOR INFORMATION.
(12)	ANY DAMAGE THAT OCCURS DURING REMOVAL SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE TDOT MANAGER.
(13)	A: REMOVED SNOWPLOWABLE PAVEMENT MARKERS TO BECOME PROPERTY OF THE CONTRACTOR. INCLUDES ALL COST ASSOCIATED WITH REMOVAL AND PROPER DISPOSAL B: REMOVAL OF EXISTING SNOWPLOWABLE PAVEMENT MARKERS FROM EXISTING CONCRETE SURFACE SHALL BE DONE IN SUCH A MANNER AS TO MINIMIZE DAMAGE TO THE ADJACENT CONCRETE. C: INCLUDES ALL COST ASSOCIATED WITH PATCHING VOID AREAS OR DIVOTS ON CONCRETE SURFACES CREATED DURING REMOVAL OF SNOWPLOWABLE PAVEMENT MARKERS WITH MATERIAL APPROVED BY THE TDOT ENGINEER.
(14)	FOR TEMPORARY PAVEMENT MARKINGS ON MILLED SURFACE AND INTERMEDIATE SURFACES.
(15)	ENHANCED FLATLINE THERMO PVM T MARKING SHALL BE APPLIED USING RIBBON METHOD AS DIRECTED BY THE TDOT PROJECT ENGINEER.
(16)	USE AN APPROVED TRACKLESS TACK FROM QPL 40-F APPLIED WITH A DISTRIBUTOR AT A MINIMUM RATE OF 0.20 GAL/SY (APPROXIMATE RESIDUAL RATE 0.10 GAL/SY). PAVING SHALL NOT BEGIN UNTIL THE DEPARTMENT IS SATISFIED THE APPLICATION RATE IS ACHIEVED AND THE EMULSION HAS FULLY BROKEN. MULTIPLE PASSES MAY BE REQUIRED. OR, EMULSION TYPE CQS-1HP MAY BE APPLIED WITH A SPRAY PAVER AT AN APPLICATION RATE BETWEEN 0.18 TO 0.23 GAL/SY. THE SPRAY PAVER SHALL BE A SINGLE PIECE OF EQUIPMENT THAT APPLIES THE TACK COAT AND SPREADS THE BITUMINOUS PAVEMENT. AT A MINIMUM THE SPRAY PAVER SHALL MEET THE PAVER REQUIREMENTS OF 407.06 AND THE DISTRIBUTOR REQUIREMENTS IN 402.03.
(17)	INCLUDES 1 TON FOR CROSS OVER AT L.M. 4.040.
(18)	INCLUDES 45 TONS TO BE USED ON MILLED SURFACES USING STANDARD APPLICATION RATE REQUIRED FOR MILLED SURFACES.
(19)	TO BE USED FOR TACK COAT UNDERNEATH OGFC. HOT APPLIED TRACKLESS TACK MAY BE EITHER HOT APPLIED ASPHALT BINDER OR AN APPROVED HOT APPLIED TRACKLESS TACK COAT FROM QPL 40, SECTION F. IF USING ASPHALT BINDER THE MINIMUM GRADE SHALL BE PG64-22 BUT A HIGHER GRADE MAY BE USED.

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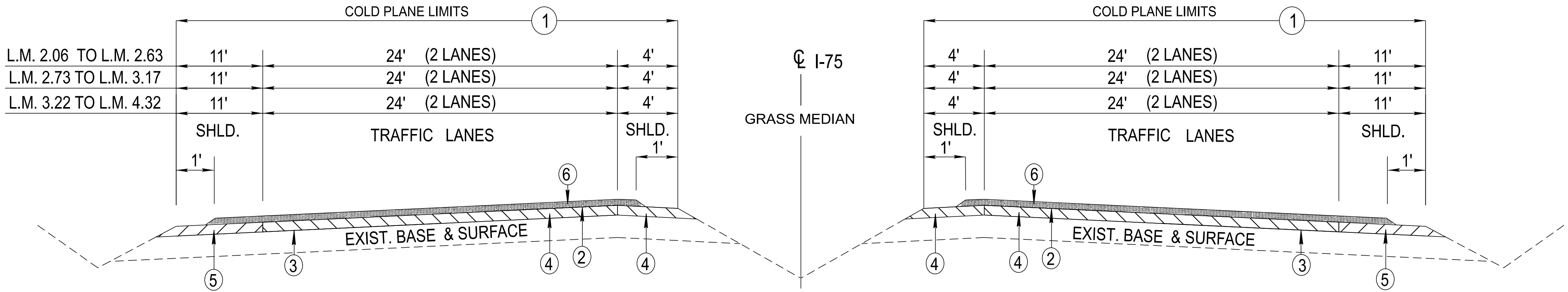


STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

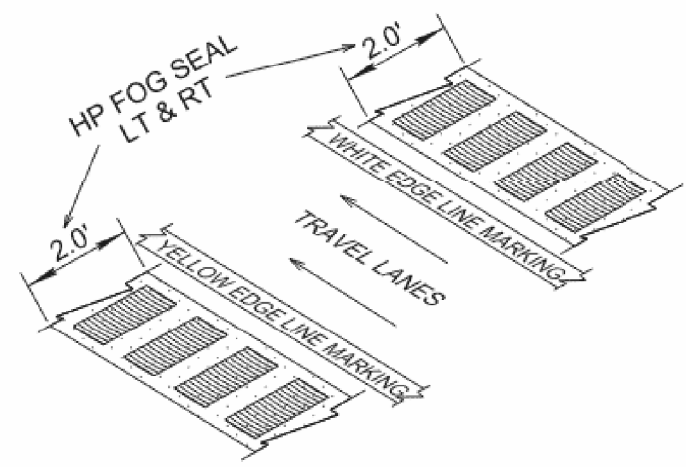
ESTIMATED  
ROADWAY  
QUANTITIES



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-75-1(159)	2B
		541075-F8-005	



PROPOSED PAVEMENT SCHEDULE				
①	COLD PLANING 1.5" TH. (APPROX. 105 LB/SY/IN) ITEM NO. 415-01.01 COLD PLANING BITUMINOUS PAVEMENT		④	BASE MIX (RDY) @ 1.5" THICK (APPROX. 165 LBS/SY) ITEM NO. 307-03.12 ASPHALT CONCRETE MIX (PG76-22) GRADING CM, TON
②	ALTERNATE "AA1"	TRACKLESS TACK COAT (TC) SEE SHEET NO. 2 FOOTNOTE (16) FOR RATE INFORMATION. ITEM NO. 403-02.01 TRACKLESS TACK COAT (TC), TON	⑤	BASE MIX (SHOULDER) @ 1.5" THICK (APPROX. 165 LBS/SY) ITEM NO. 307-01.18 ASPHALT CONCRETE MIX (PG64-22) GRADING CM, TON
	ALTERNATE "AA2"	HOT APPLIED TACK COAT (TC) @ 0.13-0.18 GAL/SY ITEM NO. 403-02.02 HOT APPLIED TACK COAT (TC), TON	⑥	ASPHALTIC CONCRETE SURFACE (HM) @ 1.25" TH. (APPROX. 117.5 LBS/SY) ITEM NO. 411-03.23 ACS MIX (PG76-22) OGFC
③	TRACKLESS TACK COAT (TC) ITEM NO. 403-02.01 TRACKLESS TACK COAT (TC), TON SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD.		⑦	ASPHALTIC CONCRETE SURFACE (HM) 1.5" TH. (APPROX. 159 LBS/SY) ITEM NO. 411-03.10 ACS MIX (PG76-22) GRADING D

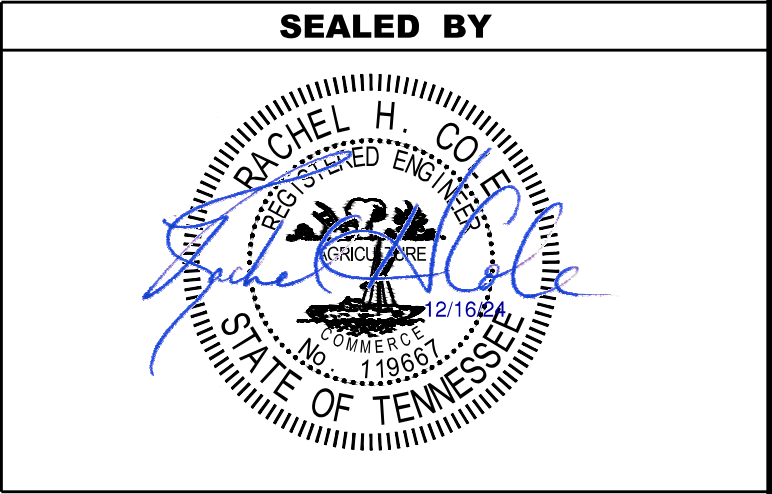
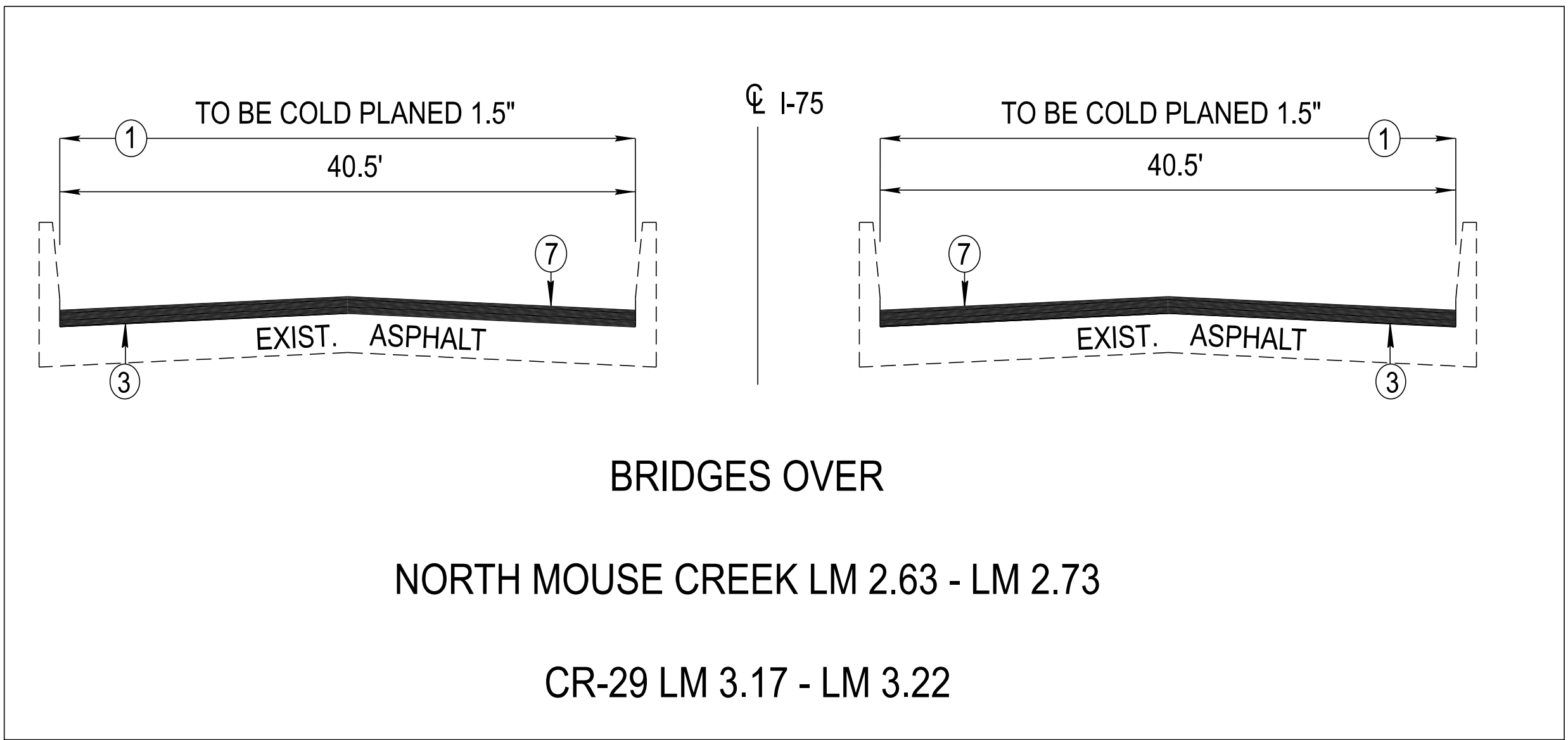
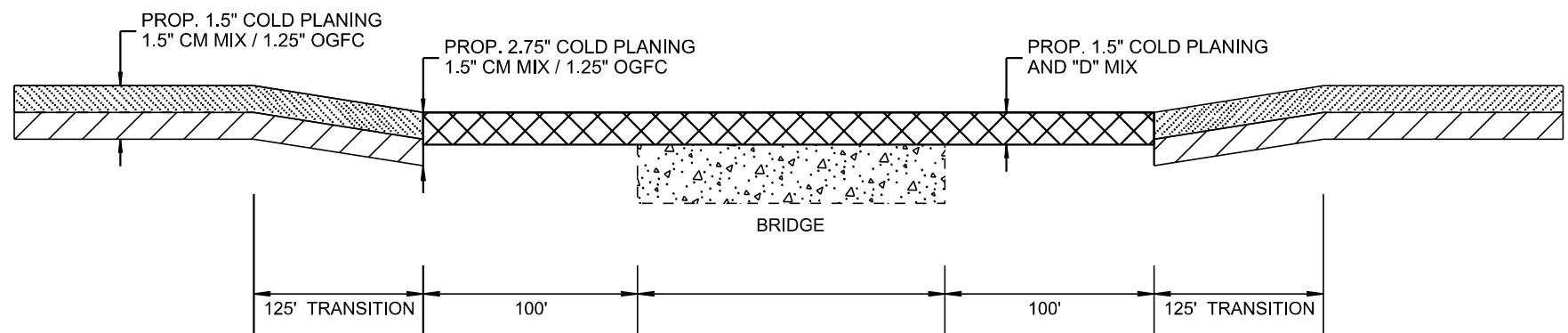
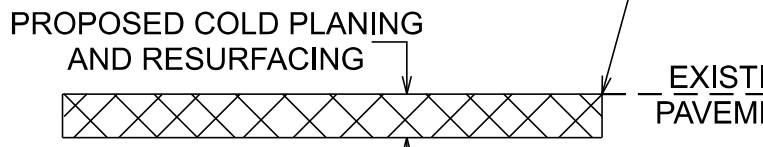


DETAIL OF FOG SEALING SCORED SHOULDERS  
ITEM 403-01.10 HIGH PERFORMANCE FOG SEAL, SY @ 0.10 - 0.15 GAL/SY  
(QUALIFIED PRODUCTS LIST QPL40.001)

THE SCORING OF SHOULDERS SHALL BE COMPLETED PRIOR TO APPLYING FOG SEAL TO PREVENT WATER INTRUSION.  
FINAL EDGE LINE STRIPING SHALL NOT BE APPLIED UNTIL HP FOG SEAL APPLICATION HAS BEEN COMPLETED.

PROFILE VIEW

THE CONTRACTOR IS TO ENSURE  
A FLUSH TRANSITION WITH  
EXISTING PAVEMENT AT  
BEGIN & END PROJECT.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE



GENERAL NOTES

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (1) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

- (7) IN AREAS WITH OPEN-GRADED FRICTION COURSE PAVEMENT, THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
- a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED IN A MANNER WHICH PERMITS PROPER DRAINAGE OF PAVEMENT STRUCTURE. MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.
  - b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

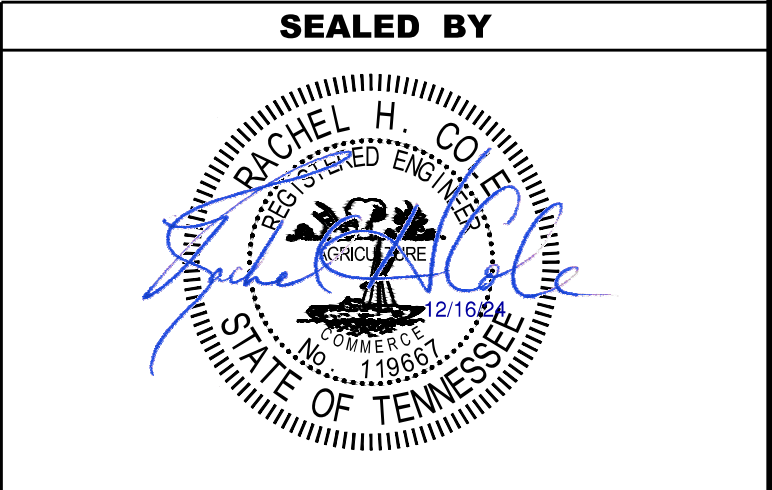
- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

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# SPECIAL NOTES

## TRAFFIC CONTROL

- (1)

THE CONTRACTOR SHALL MAKE PROVISIONS TO SAFELY CONTROL TRAFFIC INGRESS AT ALL ROADWAY ENTRANCES WITHIN THE FLAGGED WORK ZONE TO THE SATISFACTION OF THE TDOT PROJECT ENGINEER. ALL COST ASSOCIATED WITH THE CONTROLS WILL BE INCLUDED IN THE PRICE BID FOR TRAFFIC CONTROL.
- (2)

EXISTING CONSTRUCTION, REGULATORY AND WARNING SIGNS WHICH CONFLICT WITH THE CONSTRUCTION SIGNING SHALL BE REMOVED DURING CONSTRUCTION AND REINSTALLED AS DIRECTED BY THE TDOT PROJECT ENGINEER. ALL COSTS TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-01, TRAFFIC CONTROL, PER LUMP SUM.
- (3)

THE CONTRACTOR SHALL GIVE THE TDOT PROJECT ENGINEER A MINIMUM OF SEVEN (7) DAYS NOTICE PRIOR TO STARTING WORK SO THAT SUFFICIENT NOTICE CAN BE PREPARED AND DISTRIBUTED TO THE MEDIA.
- (4)

IMMEDIATELY UPON COMPLETION OF EACH CONSTRUCTION PHASE, ALL TRAFFIC CONTROL ITEMS THAT ARE NOT NECESSARY FOR THE SUCCEEDING PHASE SHALL BE REMOVED, COVERED, OR TURNED TO FACE AWAY FROM THE TRAFFIC.
- (5)

A MINIMUM OF TWO (2) TYPE "C" ARROW BOARDS WILL BE REQUIRED. IF THE CONTRACTOR SCHEDULES HIS OPERATIONS SUCH THAT MORE THAN TWO (2) ARROW BOARDS ARE REQUIRED, THE COST OF ALL ADDITIONAL ARROW BOARDS WILL BE AT THE CONTRACTOR'S EXPENSE.
- (6)

CHANGEABLE MESSAGE SIGNS UNITS (ITEM NO. 713-16.01) WILL BE REQUIRED ON THIS PROJECT. THE SIGNS WILL BE USED FOR ADVANCED WARNING OR AS DIRECTED BY THE ENGINEER.
- (7)

MESSAGE BOARDS SHALL BE DISPLAYED A MINIMUM OF SEVEN (7) DAYS PRIOR TO STARTING WORK TO INFORM PUBLIC OF UPCOMING CONSTRUCTION PROJECT.
- (8)

PRIOR TO DISPLAYING MESSAGES ON CHANGEABLE MESSAGE BOARDS, ALL MESSAGES SHALL BE APPROVED BY THE TDOT PROJECT ENGINEER AND REGION 2 TRAFFIC ENGINEER.
- (9)

INFORMATION ON CHANGEABLE MESSAGE BOARDS SHALL BE UPDATED TO REFLECT CURRENT CONSTRUCTION CONDITIONS ACTIVITIES AT ALL TIMES.
- (10)

ACCESS TO THE PROJECT WILL BE PERMITTED AT THE INTERCHANGES ONLY. THE CONTRACTOR WILL BE PERMITTED TO CROSS MEDIAN AT EXISTING CROSS-OVERS ONLY.

## LANE CLOSURES

- (1)

THE RESTRICTION OF TRAFFIC TO A TWO (2) MILE PAVING OPERATION OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (2)

THE CONTRACTOR SHALL KEEP ALL TRAFFIC LANES OPEN TO TRAFFIC DURING NON-WORKING HOURS AND/OR NON-WORKDAYS.
- (3)

DAYTIME LANE CLOSURES WILL NOT BE ALLOWED UNLESS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (4)

THE CONTRACTOR SHALL NOT BE ALLOWED TO INTERRUPT TRAFFIC FLOW AND SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION ON THE FOLLOWING DAYS:

A.

OFFICIAL STATE HOLIDAYS.

B.

FRIDAY AT 6:00 AM UNTIL TUESDAY AT 7:00 PM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON MONDAY.

C.

THURSDAY AT 6:00 AM UNTIL SUNDAY AT 7:00PM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON FRIDAY.

D.

DURING LOCAL FESTIVALS, GAMES OR EVENTS THAT COULD BE IMPEDED BY THE PAVING OPERATIONS WHERE AND AS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (5)

ALL LANE CLOSURES AND OPERATIONS MUST BE COORDINATED WITH EXISTING CONSTRUCTION CONTRACTS IN THE AREA.

## PAVING

- (1)

INTELLIGENT COMPACTION SHALL BE USED.
- (2)

ANY QUANTITY REMAINING ON ITEMS COMPLETED PRIOR TO THE PAVING OPERATION WILL NOT BE CONVERTED TO ADDITIONAL ASPHALT FOR THE ROADWAY.

## PAVEMENT MARKING

- (1)

THE CONTRACTOR IS RESPONSIBLE FOR THE LAYOUT OF ALL PAVEMENT MARKING. ANY CHANGE FROM THE EXISTING PAVEMENT MARKINGS MUST BE APPROVED BY THE REGION 2 TRAFFIC MANAGER.
- (2)

RAMPS SHALL BE MARKED UP TO WHERE THEY CONNECT TO THE INTERSECTING ROADWAY.
- (3)

ENHANCED FLATLINE THERMO PVMT MARKING SHALL BE APPLIED USING RIBBON METHOD AS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (4)

EDGE LINE AND LANE LINE MARKINGS ON CONCRETE SURFACE OF INTERCHANGE RAMPS SHALL BE CONTRAST MARKINGS.
- (5)

EXISTING MARKINGS ON CONCRETE SURFACE OF INTERCHANGE RAMPS SHALL BE REMOVED BY HYDROBLAST METHOD.

## OGFC

- (1)

AT THE START OF EACH NIGHT'S PAVING, PRODUCE APPROXIMATELY 15 TONS OF THE BITUMINOUS PAVEMENT FOR THE PURPOSE OF PLANT START UP AND HEATING UP THE MATERIAL TRANSFER DEVICE (MTD). THIS MATERIAL SHALL BE UNLOADED INTO THE MTD AT A MINIMUM TEMPERATURE 280f AND THE ENTIRE LOAD DISCHARGED THROUGH THE MTD AND WASTED OFF THE PROJECT SITE IMMEDIATELY PRIOR TO THE COMMENCING OF PAVING OPERATIONS.

## COLD PLANING

- (1)

IF SUBSURFACE PAVEMENT FAILURE IS EXPOSED AFTER MILLING, REMOVE AREA OF FAILURE TO SOUND PAVEMENT AND REPLACE WITH "BM2" MIX PRIOR TO PLACING PROPOSED OVERLAY.
- (2)

AT THE COMPLETION OF COLD PLANING OPERATION, ANY DEPRESSIONS WILL BE CLEANED AND BROUGHT TO PROPER GRADE WITH "BM2" MIX MATERIAL. THE COST OF THE CLEAN UP WILL BE INCLUDED IN THE UNIT PRICE FOR "BM2" MIX MATERIAL.

## PAVEMENT

- (1)

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:

A.

THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.

B.

THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.

C.

THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS ≥ 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS < 70,000.

D.

RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.

E.

ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS

F.

IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.

G.

ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.
- (2)

ALL DRAINS ACROSS BRIDGES NEED TO BE CHECKED AND CLEANED AFTER PAVING IS COMPLETE. COST TO BE INCLUDED IN OTHER ITEMS.
- (3)

THE INSIDE SHOULDER SHALL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.

## NIGHT WORK

- (1)

ALL COST ASSOCIATED WITH NIGHT WORK ZONE LIGHTING SHALL BE INCLUDED IN ITEM 712-01.
- (2)

THE CONTRACTOR SHALL COMPLY WITH SECTION 712.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING NIGHT WORK LIGHTING.
- (3)

LIGHTS FOR NIGHT WORK TO BE APPROVED BY THE TDOT ENGINEER.
- (4)

NIGHT WORK SHALL BE DONE BETWEEN THE HOURS OF 7:00 PM AND 6:00 AM SUNDAY NIGHT THROUGH SATURDAY MORNING. ALL WORK TO BE DONE AT NIGHT SHALL BE DURING THESE HOURS. THIS INCLUDES CONSTRUCTION, MOBILIZATION OF EQUIPMENT AND PLACEMENT AND REMOVAL OF TRAFFIC CONTROL DEVICES.
- (5)

ALL CONSTRUCTION SIGNS THAT ARE TO REMAIN IN PLACE DURING CONSTRUCTION MAY BE INSTALLED DURING DAYTIME HOURS AS DIRECTED BY THE TDOT PROJECT ENGINEER.

## FOG ZONE

- (1)

IN THE FOG ZONE AREA, THE SNOWPLOWABLE MARKERS SHALL BE PLACED ON A SPACING OF 20' CENTER TO CENTER ALONG THE BROKEN LINES, THE SPACING SHALL BE 40' CENTER TO CENTER ALONG THE EDGE OF PAVEMENT. THEN BACK TO NORMAL SPACING IN THE NON-FOG ZONE SPECIALTY MARKING AREA. (SEE MUTCD SECTION 3F.04 FOR SPACING.)
- (2)

ROADWAY DELINEATORS:

•

IN THE FOG ZONE THE FLEXIBLE DELINEATOR POST SHALL BE BROWN, INSTALLED WITH A POST SPACING OF 40 LF.

•

THE REFLECTIVE SHEETING SHALL BE 3 INCHES X 9 INCHES MANUFACTURED BY RFELEXITE-SILVER (CANDLE POWER 1472) OR APPROVED EQUAL.

•

ALL COST ASSOCIATED WITH REMOVING ALL DAMAGED FLEXIBLE DELINEATORS AND INSTALLING NEW BROWN FLEXIBLE DELINEATORS TO BE PAID FOR UNDER ITEM NO. 713-02.14.

•

THE REMOVAL OF THE EXISTING FLEXIBLE DELINEATORS SHALL BE RESTRICTED TO THE IMMEDIATE WORK ZONE THAT IS CLOSED TO TRAFFIC.

•

THE FLEXIBLE DELINEATORS SHALL BE INSTALLED AS SOON AS POSSIBLE AFTER THE SHOULDER STONE PLACEMENT.

## END OF TRAFFIC QUEUE PROTECTION

- (1)

WHEN INTERSTATE LANE CLOSURES ARE PERFORMED, THE CONTRACTOR SHALL PROVIDE A MINIMUM OF TWO (2) TRUCK MOUNTED MOBILE CHANGEABLE MESSAGE SIGNS WITH TRUCK MOUNTED ATTENUATORS FOR EACH TRAVELING DIRECTION IF WHICH LANES ARE CLOSED, TO PROTECT THE END OF THE TRAFFIC QUEUE

A.

AT LEAST ONE UNIT SHALL BE POSITIONED ON THE SHOULDER UPSTREAM FROM THE END OF THE TRAFFIC QUEUE AT ALL TIMES, BUT NO FURTHER THAN 1/2 MILE FROM THE QUEUE.

B.

THE CHANGEABLE MESSAGE SIGN SHALL DISPLAY TWO PHASES: "STOPPED TRAFFIC AHEAD" AND "BE PREPARED TO STOP"

C.

IN THE EVENT THAT THE TRAFFIC QUEUE EXTENDS UPSTREAM BEYOND THE UNIT, A SECOND UNIT SHALL MOBILIZE IN ACCORDANCE WITH [A.]. THE DOWNSTREAM UNIT MAY DEPART WHEN THE SECOND UNIT HAS MOVED INTO POSITION.

D.

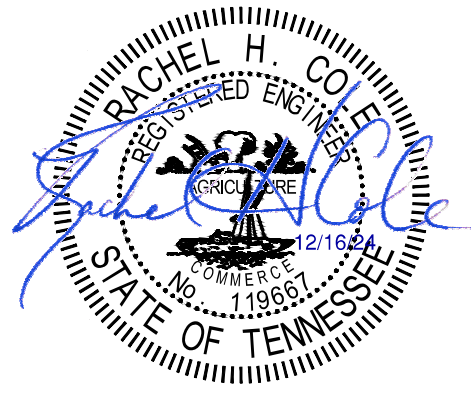
TRUCK MOUNTED CHANGEABLE MESSAGE SIGNS AND ATTENUATORS SHALL BE IN ACCORDANCE WITH THE NCHRP REPORT 350, MASH TL-3, MUTCD, AND MOUNTED PER THE MANUFACTURER'S RECOMMENDATIONS. TRUCK MOUNTED CHANGEABLE MESSAGE SIGNS SHALL BE LISTED ON QPL LIST 30, SECTION B, AND TRUCK MOUNTED ATTENUATORS SHALL BE LISTED ON QPL LIST 34, SECTION D.

E.

MOBILE CHANGEABLE MESSAGE SIGN UNITS WITH MOUNTED ATTENUATOR WILL BE PAID FOR AS ITEM NO.712-08.12, QUEUE PROTECTION TRUCK, DAY ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID.

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# ENVIRONMENTAL NOTES

## ENVIRONMENTAL GENERAL NOTES

### NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

### SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

### PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

## ENVIRONMENTAL SPECIAL NOTES

### ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

### SCOPE OF WORK

- (6) NIGHT WORK, COLD PLANING, GR CM MIX, OGFC, INTELLIGENT COMPACTION, AND PAVEMENT MARKINGS.

## EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

### DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

### SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING

PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

### GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

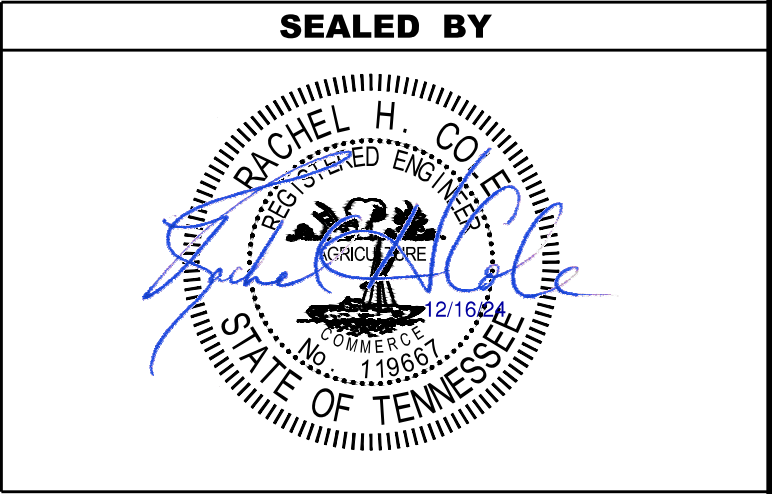
- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED

TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

### SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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RESURF.	2025	NH-I-75-1(159)	2F
		541075-F8-005	

BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
54I00750035	2.20	CR-28		TO MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
54I00750004	2.67	NORTH MOUSE CREEK	210'	COLD PLANE 1.5" OF THE EXISTING ASPHALT AND REPLACE WITH 1.5" OF NEW ASPHALT.
54I00750003	2.69	NORTH MOUSE CREEK	234'	COLD PLANE 1.5" OF THE EXISTING ASPHALT AND REPLACE WITH 1.5" OF NEW ASPHALT.
54I00750005	3.19	CR-29	90'	COLD PLANE 1.5" OF THE EXISTING ASPHALT AND REPLACE WITH 1.5" OF NEW ASPHALT.
54I00750006	3.20	CR-29	90'	COLD PLANE 1.5" OF THE EXISTING ASPHALT AND REPLACE WITH 1.5" OF NEW ASPHALT.

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES			S.F.	TOTAL NUMBER REQUIRED
		L	x	W		
G20-1	ROAD WORK NEXT 3 MILES	64"	x	24"	11	2
G20-2	END ROAD WORK	48"	x	24"	8	2
W1-4AR	REVERSE CURVE (RT)	48"	x	48"	16	2
W4-2R	LANE ENDS (RT)	48"	x	48"	16	2
W4-2L	LANE ENDS (LT)	48"	x	48"	16	2
W8-11	UNEVEN LANES	48"	x	48"	16	24
W8-15	GROOVED PAVEMENT	48"	x	48"	16	24
W8-15P	MOTORCYCLE (PLAQUE)	24"	x	18"	3	24
W8-17	SHOULDER DROP OFF (SYMBOL)	48"	x	48"	16	24
W8-17P	SHOULDER DROP-OFF (PLAQUE)	24"	x	18"	3	24
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	2
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	2
W20-1	ROAD WORK 1000 FT	48"	x	48"	16	2
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x	48"	16	2
W20-5R	RIGHT LANE CLOSED 1500 FT	48"	x	48"	16	2
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x	48"	16	2
W20-5L	LEFT LANE CLOSED 1500 FT	48"	x	48"	16	2
W21-5R	RIGHT SHOULDER CLOSED 1500 FT	48"	x	48"	16	2
W21-5R	RIGHT SHOULDER CLOSED	48"	x	48"	16	2
W21-5L	LEFT SHOULDER CLOSED 1500 FT	48"	x	48"	16	2
W21-5L	LEFT SHOULDER CLOSED	48"	x	48"	16	2
TOTAL						1771

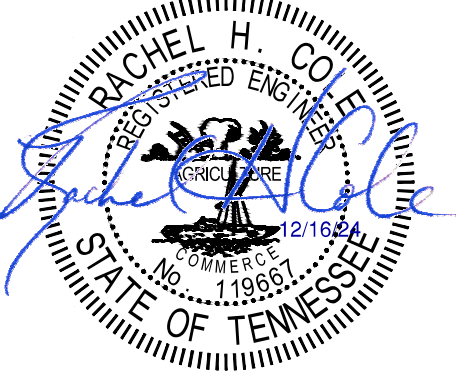
McMinn Co. I-75 CROSS-OVERS				
LOCATION (ROADWAY)	AREA (SY)	REPAIR WORK		
		403-02.01 TRACKLESS TACK COAT TON	307-01.08 ACS MIX (PG64-22) GR "CM" TON	COMMENTS
LM 4.040	1325.0	1.0	120.0	PAVE 8' INSIDE SHOULDER FOR 100' EITHER SIDE OF CROSSOVER
TOTALS		1.0	120.0	

UTILITY ADJUSTMENTS (RESURFACING)
THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

STORM DRAIN ADJUSTMENTS (RESURFACING)
THERE ARE NO STORM DRAIN ADJUSTMENTS ON THIS PROJECT

PROPOSED GUARDRAIL (RESURFACING)
THERE ARE NO GUARDRAIL ADJUSTMENTS ON THIS PROJECT

SEALED BY



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES

UTILITY

- (1)

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER “THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT”, THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

**CABLE & FIBER:**  
**CHARTER COMMUNICATIONS**  
1103 S Hamilton Street  
DALTON, GA 30720  
CONTACT: BILLY CHADWICK  
OFFICE PHONE: 706 783 7925  
Email: billy.click@charter.com

**ELECTRIC:**  
**ATHENS UTILITIES BOARD**  
PO BOX 689  
ATHENS, TN 37371-0689  
CONTACT: ERIC NEWBERRY  
OFFICE PHONE: 423 745 4501  
Email: enewberry@aub.org

**FIBER:**  
**ATHENS UTILITIES BOARD**  
PO BOX 689  
ATHENS, TN 37371-0689  
CONTACT: ERIC NEWBERRY  
OFFICE PHONE: 423 745 4501  
Email: enewberry@aub.org

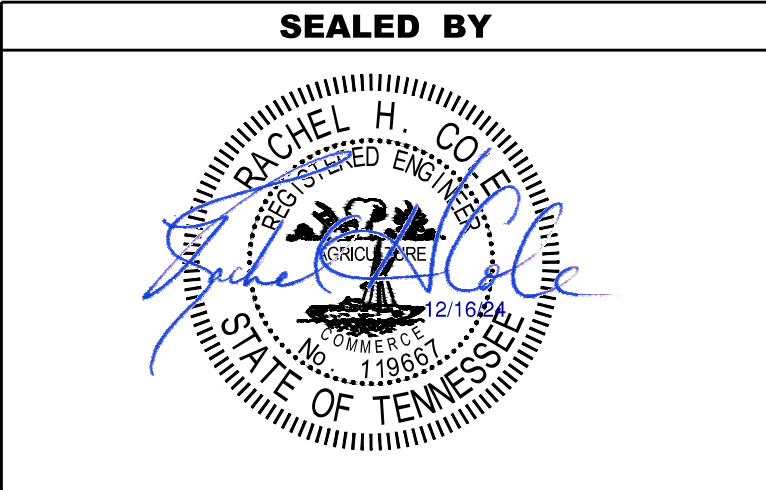
**GAS:**  
**ATHENS UTILITIES BOARD**  
PO BOX 689  
ATHENS, TN 37371-0689  
CONTACT: ERIC NEWBERRY  
OFFICE PHONE: 423 745 4501  
Email: enewberry@aub.org

**GAS:**  
**COLONIAL PIPELINE COMPANY**  
391 SCRUGGS ROAD  
RINGGOLD, GA 30736  
CONTACT: SCOTTY DAVIS  
OFFICE PHONE: 706 891 7584  
Email: sdavis@colpipe.com

**SEWER:**  
**ATHENS UTILITIES BOARD**  
PO BOX 689  
ATHENS, TN 37371-0689  
CONTACT: ERIC NEWBERRY  
OFFICE PHONE: 423 745 4501  
Email: enewberry@aub.org

**WATER:**  
**ATHENS UTILITIES BOARD**  
PO BOX 689  
ATHENS, TN 37371-0689  
CONTACT: ERIC NEWBERRY  
OFFICE PHONE: 423 745 4501  
Email: enewberry@aub.org

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-L-75-1(159)	3
		541075-F8-005	



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES  
AND  
UTILITY OWNERS



PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

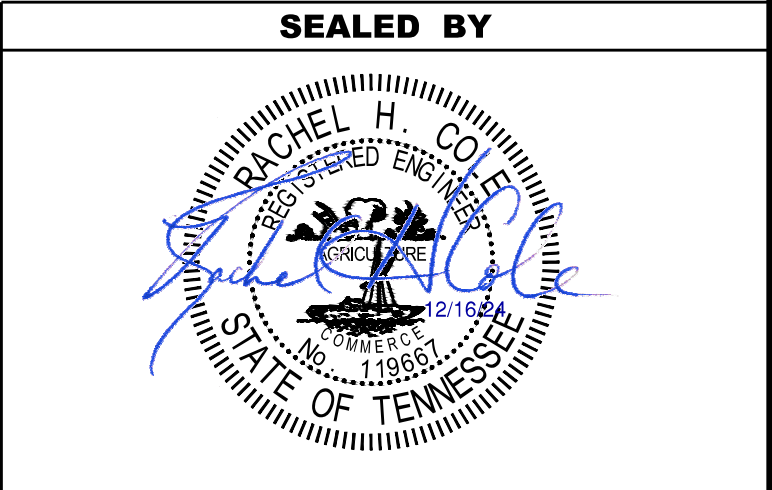
(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-L-75-1(159)	T1
		541075-F8-005	



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE  
DROP-OFF NOTES  
FOR  
TRAFFIC CONTROL



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-75-1(159)	T2
		54I075-F8-005	

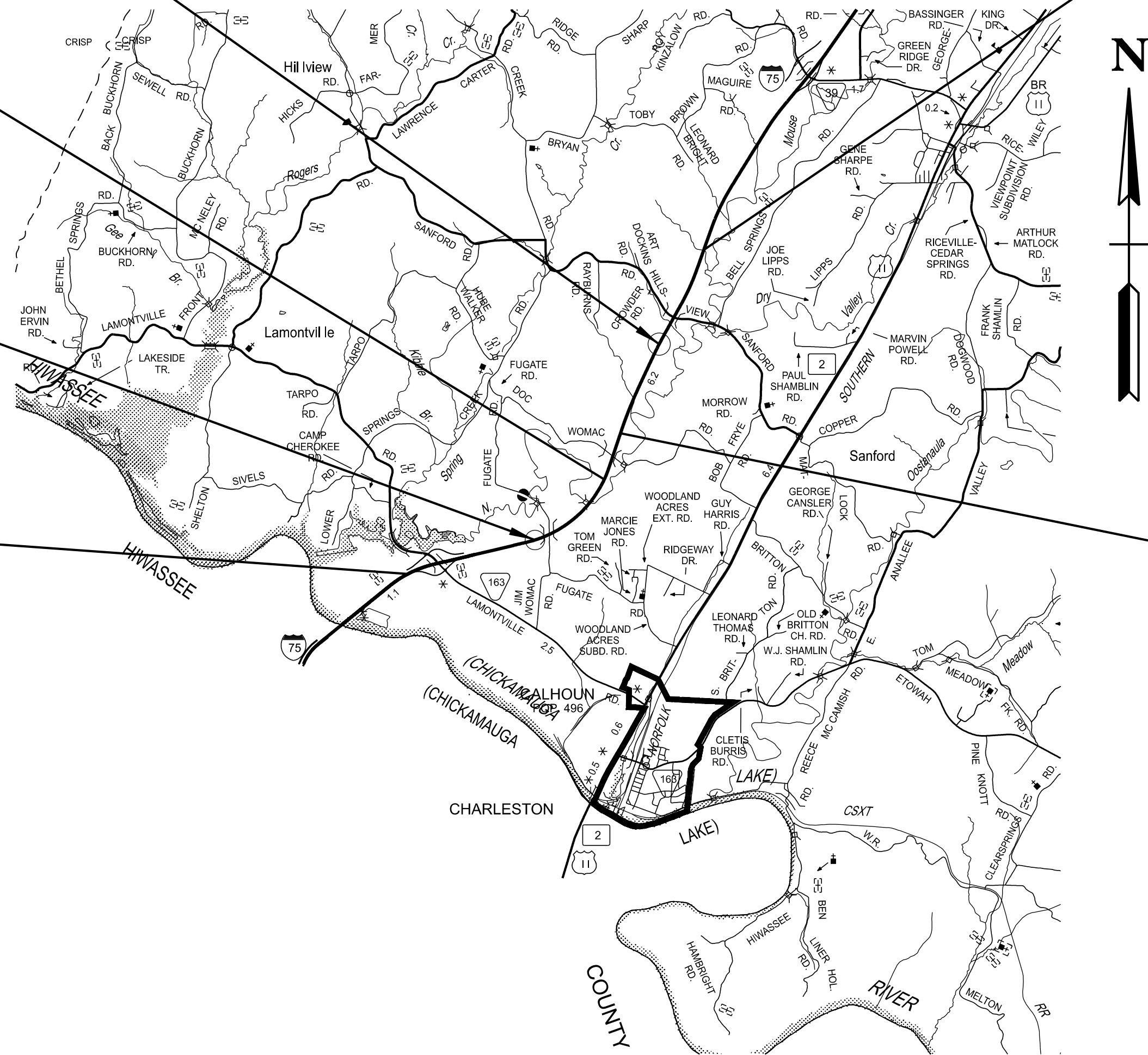
54I075-F8-005  
END PROJECT NO. NH-I-75-1(159) RESURFACE  
M.M. #39.17 L.M. 4.32

DIGITAL SPEED LIMIT ASSEMBLY  
APPROX. LOCATION LM 5.32 (I-75 SB)  
(1 MILE AFTER END PROJECT)

DIGITAL SPEED LIMIT ASSEMBLY  
APPROX. LOCATION LM 3.06 (I-75 NB)  
(AT APPROX. 2 MILE SPACING)

54I075-F8-005  
BEGIN PROJECT NO. NH-I-75-1(159) RESURFACE  
M.M. #36.94 L.M. 2.06

DIGITAL SPEED LIMIT ASSEMBLY  
APPROX. LOCATION LM 1.06 (I-75 NB)  
(1 MILE PRIOR TO BEGIN PROJECT)

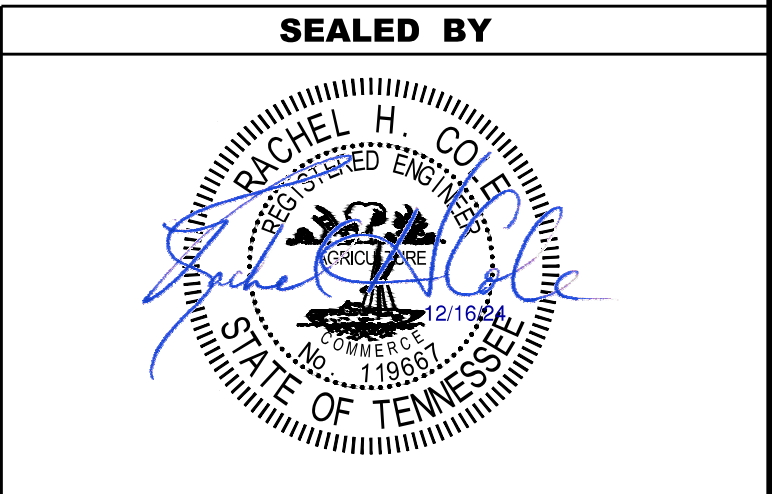


SCALE: 1"= 5280'  
0 1 2 3 MILES

#### DETAIL OF SUGGESTED LOCATIONS OF DIGITAL SPEED LIMIT ASSEMBLY

- (A) THE CONTRACTOR SHALL MONITOR ALL SPEED LIMIT ASSEMBLIES TO PREVENT COMMUNICATING CONFLICTING MESSAGES CONCERNING THE SPEED LIMIT.
- (B) EXISTING SPEED LIMIT SIGNS SHALL BE SUFFICIENTLY COVERED THROUGHOUT THE ENTIRE WORK ZONE. IF THE COVERING BEGINS TO REVEAL THE EXISTING SPEED LIMIT SIGN, THE COVERING SHALL BE REPAIRED OR REPLACED AS DIRECTED BY THE TDOT PROJECT ENGINEER. SEE SPECIAL PROVISION 712DSL.
- (C) TAPE SHALL NOT BE AFFIXED TO THE REFLECTIVE SHEETING.

THE ESTIMATED TIME REQUIRED FOR THE QUEUE TRUCK PROTECTION IS 500 HOURS.  
TO BE PAID FOR UNDER ITEM 712-08.12 QUEUE PROTECTION TRUCK, 42 DAYS TOTAL.  
SEE SPECIAL PROVISION 712PTQ FOR INFORMATION.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TRAFFIC  
CONTROL  
PLANS